

PLANNING AND DEVELOPMENT SERVICES STAFF REPORT

For Planning Commission Meeting of March 18, 2014

SUBJECT: **South State Street Shell Gas Station/Drive-Thru Restaurant Site Plan for City Council Approval
(2991 S. State Street)
File No. SP13-058**

PROPOSED CITY PLANNING COMMISSION MOTION

The Ann Arbor City Planning Commission hereby recommends that the Mayor and City Council approve the South State Street Shell Gas Station/Drive-Thru Restaurant Site Plan, subject to installation of a fire hydrant prior to the issuance of any building permits.

PROPOSED CITY PLANNING COMMISSION MOTION

The Ann Arbor City Planning Commission hereby recommends the Mayor and City Council approve the proposed landscape modifications according to Chapter 62 (Landscape and Screening Ordinance), Section 5:608 (2)(c) (ii).

STAFF RECOMMENDATION

Staff recommends that the site plan be **approved** because, if the proposed landscape modifications are approved, the plan would comply with all local, state and federal laws and regulations; the development would limit the disturbance of natural features to the minimum necessary to allow a reasonable use of the land; and the development would not cause a public or private nuisance and would not have a detrimental effect on public health, safety or welfare.

Staff recommends that the proposed landscape modification be **approved** because the changes are consistent with the intent of the ordinance and are associated with a previously approved site plan.

LOCATION

This site is located at the northeast corner of the East Eisenhower Parkway and South State Street intersection (South Area and Malletts Creek Watershed).

DESCRIPTION OF PETITION

The petitioner proposes to demolish the existing one-story convenience building and car wash on this gas station site, totaling 2,435-square feet, and construct a single building containing a 1,250-square foot drive-thru restaurant and 3,000-square foot convenience store. The existing gas pump island canopy will remain. Total estimated cost of project is \$800,000.

The single lane drive-thru will be primarily accessed from the existing E. Eisenhower Parkway curb cut, with vehicles looping in an east to north direction before exiting onto either S. State Street or E. Eisenhower Parkway. The drive-thru lane provides stacking for up to 9 vehicles and will be screened to the west by the proposed building.

Based on previous variance conditions, the southern curb cut off S. State Street and the curb cut on E. Eisenhower Parkway will continue to allow for right turn in and right turn out for this site. The northern curb cut off S. State Street has no restrictions on ingress and egress to this site. A new sidewalk connection with striped crosswalk is proposed to connect the southern building entrance to the public sidewalk along E. Eisenhower Parkway.

A total of 22 parking spaces are proposed for the restaurant, convenience store and gas station uses. Eight of these parking spaces are located at the four gas pump islands. Three Class C bicycle hoops are located near the south building entrance. These three hoops are located near a bio-retention island and could accommodate up to 6 spaces if needed. The petitioner is counting the required 3 northerly facing spaces since the bio-retention side of the hoops may not be utilized. A dumpster and recycling carts are located at the northeast corner of the site.

100-year storm water detention is required and is located under the parking lot. The storm system proposed is designed with an open bottom to allow infiltration, however since the soils are not favorable for infiltration, volume within the basin will be provided for the full 100-year storm. Bioswales are proposed in the landscape islands around the building perimeter and will receive water from roof run-off.

The existing 4-foot wide right-of-way landscape buffer strip fronting S. State Street contains a 30-inch high screening wall and landscaping. The petitioner is requesting approval of a landscape modification to maintain the reduced width of the buffer, allowing for continued use of the two gas pump islands located on the western portion of this site. In exchange for this reduced buffer width, additional landscaping is proposed within the S. State Street and E. Eisenhower Parkway buffers to further screen the vehicular use area.

The existing gas pump islands and convenience store are proposed to remain in operation during construction of the new convenience store and drive-thru restaurant building. Once finished, the two eastern pumps are to be relocated near the existing west side pumps and the existing convenience store is to be removed.

A post card was sent to neighbors within 500 feet of this site notifying them of this project. At the time this staff report was written, the petitioner and the City have not received any comments or concerns from the public regarding this proposal.

SURROUNDING LAND USES AND ZONING

	LAND USE	ZONING
NORTH	Office	O (Office District)
EAST	Office	O
SOUTH	Office	O
WEST	Financial Institution and Mixed Uses	PUD (Planned Unit Development)

COMPARISON CHART

	EXISTING	PROPOSED	REQUIRED/PERMITTED
Zoning	C3 (Fringe Commercial District)	C3	C3
Gross Lot Area	27,686 sq ft	27,686 sq ft	6,000 sq ft MIN
Floor Area in Percentage of Lot Area	2,435 sq ft 8.8%	4,250 sq ft 15.4%	55,372 sq ft 200% MAX
Setback – Front (E. Eisenhower Pkwy)	19.87 ft	25 ft	10 ft MIN** 25 ft MAX
Setback – Front (S. State St.)	7 ft*	105.2 ft	10 ft MIN** 25 ft MAX
Setback - Side	2.5 ft	15 ft	None
Setback – Rear	48.3 ft	34.8 ft	None
Height	1 story 12 ft	1 story 30 ft	4 stories MAX 55 ft MAX
Parking – Automobile	14 spaces	22 spaces	22 spaces MIN***
Parking – Bicycle	2 spaces – Class C	6 spaces – Class C	3 spaces MIN – Class C

*Variance approved for setback to canopy

**Maximum setbacks shall apply to at least 1 lot line for new freestanding buildings on parcels with more than 1 front lot line

*** 8 parking spaces counted at pump islands

HISTORY

The building was developed in Pittsfield Township in 1968, annexed into the City in 1971 and remained unzoned until 1982 when an area zoning study was completed and a C3 (Fringe Commercial) zoning was approved.

A site plan was approved in August 1985, providing for demolition of an existing structure and for construction of convenience store and carwash buildings. This site plan was developed in coordination with the City's State and Eisenhower intersection improvements. As part of that site plan approval, a variance was granted in July 1985 to allow a setback of seven feet (40 feet required) from the State Street property line for the canopy structure. Planning staff at that time indicated support because of the additional right-of-way that was being acquired by the City along S. State Street. A variance was also granted to permit a third curb cut for the site, with the conditions that the southerly driveway on State Street be one-way and reduced in width and that the divided drive at E. Eisenhower Parkway be reduced to a 30 foot opening.

An administrative amendment was approved in May 1995 to add two additional service islands; however, this plan was never implemented. An administrative amendment was approved in 1997 for a 110-foot equipment room to the west side of the carwash, which was constructed.

PLANNING BACKGROUND

The South State Street Corridor Plan recommends commercial uses for this site. The Master Plan: Land Use Element recommends that retail centers be designed with pedestrian access as a primary goal, which encourages mass transit and reduces the demand for parking. Surface parking should be placed at the rear of the building and should not be a dominant element along public streets.

Providing extensive landscaping can soften the visual impact of development, offer shade to encourage pedestrian modes of travel, screen adjacent uses from parking lot lighting, improve property values, and help reduce energy costs.

The Non-Motorized Plan recommends bicycle lanes and sidewalks fronting S. State Street and a shared use path fronting Eisenhower Parkway. These lanes and paths exist.

The Transportation Plan considers this site a high priority corridor. The city is currently undertaking a study to determine appropriate transportation improvements on S. State Street between Oakbrook Drive and Ellsworth Road. This portion of S. State Street is being considered as a potential leg as part of the Connector Study, which is looking at high capacity transit options connecting the south part of the city through downtown and Central Campus to the Plymouth Road corridor.

LANDSCAPE MODIFICATION: STANDARDS FOR APPROVAL

Flexibility in the application of the landscape and screening regulations may be allowed if certain standards are met. The modifications must be consistent with the intent of Chapter 62; be included on a site plan and in a motion approved by the City Planning Commission or City Council; and be associated with specific site conditions as listed in Section 5:608(2)(c).

Petitioner responses in regular text, staff comments in *italics*

1. What are the specific site conditions that necessitate this request and how do they warrant the modifications of Chapter 62 requirements? (See Section 5:608(2)(c)) List relevant subsection and explain how and to what extent the modifications are justified.

This modification is necessary because strict application of this Chapter will result in the loss of existing parking spaces at the existing gas pump island that is proposed to be unchanged as part of this site plan submission. These parking spaces do not abut residential.

A minimum 10-foot wide right of way buffer strip is required to screen from the vehicular use area. Currently the site has a 4-foot wide buffer strip, with a 30-inch screening wall and landscaping. This proposal adds landscaping in exchange for the 6-foot reduction in buffer width. Widening the buffer width would eliminate two existing pumps and their associated parking spaces.

2. How does the proposal meet the spirit and intent of Chapter 62? (See Section 5:600)

This proposal will save existing required parking spaces, allow for less congested flow of traffic within the site, hence improve the quality and safety of pedestrian movement within paved areas and along the public right-of-way. It will also protect and preserve the appearance, character and value of the surrounding neighborhood.

Landscaping beyond the required amount is being proposed. 33 shrubs required and 41 proposed; 11 trees required and 18 proposed.

The intent of the ordinance is to provide a screening buffer between the parking lot and the public right-of-way. A reduced buffer strip to 4-feet wide, along with additional landscaping and a 30-inch high wall screens the vehicular use area from S. State Street.

STAFF COMMENTS

Traffic – Staff agrees with the conclusions of the traffic report for this site. The proposed drive-thru restaurant generates approximately 60 customer trips during the morning peak hour, with half anticipated to use the drive thru lane. These customers will primarily be traffic that is already driving by the site. The amount of anticipated traffic is not expected to cause traffic back-ups. The elimination of the car wash reduces afternoon and evening trips to this site.

Project Management - A City of Ann Arbor Geodetic Reference System (AAGRS) Coordinate Transformation Worksheet shall be submitted for review and approval. If footing drains for the existing building are connected to the sanitary sewer system, disconnection will be required in accordance with current city specifications.

Malletts Creek Coordinating Committee Recommendation: According to the Rules of the Washtenaw County Water Resources Commissioner, in commercial and industrial developments where large amounts of oil, grease, petroleum, or other polluting materials may accumulate, measures will be employed to reduce and trap pollutants prior to discharge into a storm water detention system. The Draft Rules of the WCWRC are even clearer stating, “gas stations require hydrocarbon filters in all catch basins on the site.” As such, please include hydrocarbon filters in all catch basins on the site and consider installing an oil/water separator within the proposed storm water system prior to water exiting the site.

Systems Planning – Storm Water – The storm outlet structure will function similar to an oil/water separator and hydrocarbon filters were added to all catch basins to meet the recommendations of the Malletts Creek Coordinating Committee.

Systems Planning – Forestry – Staff supports the landscape modification submitted by the petitioner to maintain the right of way buffer width of 4 feet along S. State Street due to a loss of required parking spaces if the landscape requirement were met. The petitioner is proposing more landscaping in the right-of-way buffers along both Eisenhower Parkway and S. State Street than is required.

Planning – The Master Plan: Land Use Element does not recommend drive thrus and service stations but supports commercial uses with strong pedestrian connections. The Plan states that redeveloping retail centers should be designed with pedestrian access as a primary goal. Designing commercial centers with safe, convenient pedestrian routes will encourage pedestrians to walk from surrounding neighborhoods and employment centers.

The C3 zoning district currently allows drive-thru facilities as a permitted use. The petitioner has designed the site with a pedestrian connection off Eisenhower Parkway, along with bicycle parking near the southern building entrance. The drive-thru lane is screened from S. State Street by the building and from Eisenhower by landscaping. Although the drive-thru restaurant

is expected to nominally increase daily traffic to this site, most of this traffic is existing drive-by trips during the morning hours.

Staff research shows the current right-of-way landscape buffer along State Street was originally site planned for a 5 foot width, which was a permitted exception for gas stations in the Landscape Ordinance in effect at the time. It is likely the planned five foot width was reduced to four feet sometime later in the recording of the new right-of-way. Staff supports the landscape modification request to reduce the buffer strip width from 10 feet to 4 feet. Increasing the buffer width to 10 feet would require the petitioner to relocate the two associated parking spaces and make the existing pump islands less accessible. The petitioner proposes additional landscaping to screen the vehicular use area and site from the public right-of-way in exchange for this reduction in width.

Prepared by Chris Cheng
Reviewed by Wendy Rampson
mg/3/14/14

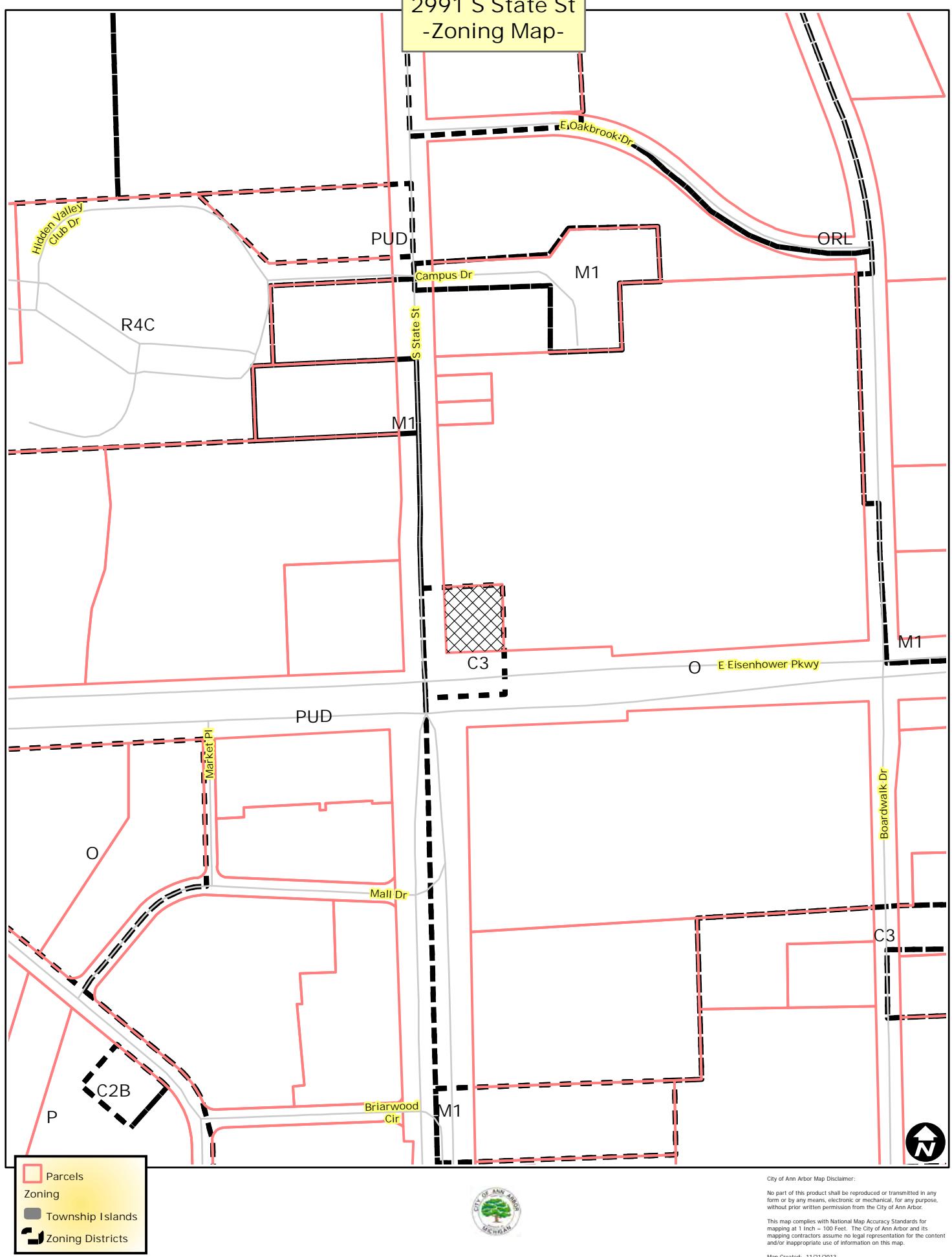
Attachments: Zoning/Parcel Maps
Aerial Photo
Site Plan
Landscape Plan
Elevations

c: Petitioner/Owner: Joseph Kafi
JAK Cubed, LLC
2991 S. State Street
Ann Arbor, MI 48103

Petitioner's Representative: Brad Cousino, PE
Terratek Desgin, Inc.
2860 Tepeyac Hill Dr.
Ann Arbor, MI 48105

Project Management
Systems Planning
File No. SP13-058

2991 S State St -Zoning Map-



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2991 S State St -Aerial Map-



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Map Created: 11/21/2013



Parcels

2991 S State St
-Aerial Map-



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Map Created: 11/21/2013



Parcels

SITE PLAN

Proposed 4250 Sq. Ft. Retail and Restaurant Shell Service Station & Convenience Store 2991 South State Street, Ann Arbor MI

OWNER

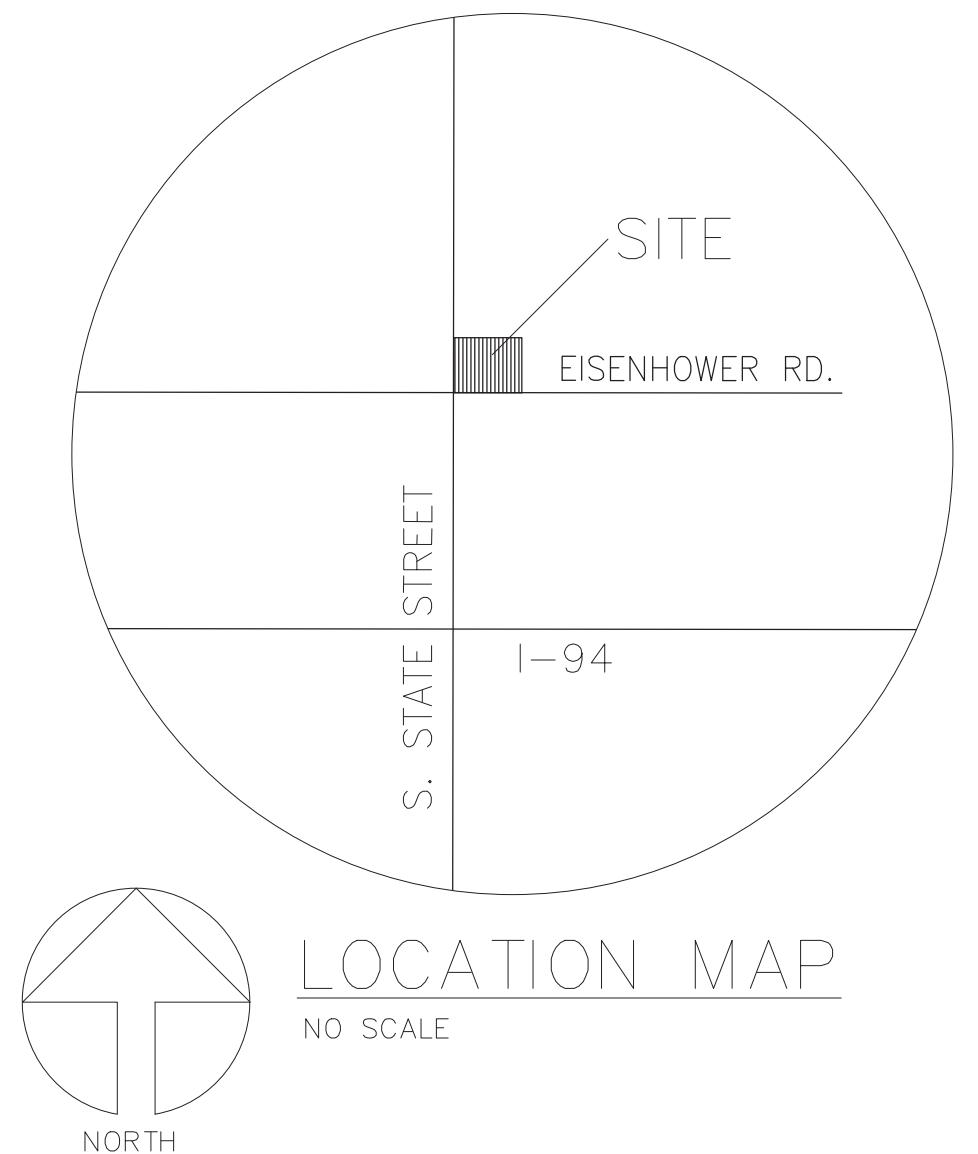
JAK Cubed, LLC
2991 S. State St.
Ann Arbor, MI 48104

CIVIL ENGINEER

TERRATEK DESIGN, INC.
BRADLEY J. COUSINO, P.E.
2860 TEPEYAC HILL DR.
ANN ARBOR, MI 48105
734.320.4626

ARCHITECT

Zack M. Ostroff & Assoc.
www.zoarchitecture.com
ph. 248.425.4190
fx. 248.366.7122



REQUIRED STATEMENTS

DEVELOPMENT PROGRAM

The proposed land use is the same as current and consistent with C-3 Zoning. The site will continue as a gasoline filling station and convenience store, the additional feature will be a 1250 sq ft restaurant with a drive-thru window and limited inside seating. The existing convenience store with offices and drive-thru auto wash are to be demolished. It is the intention of the owners to remain operational during the proposed improvements with the possibility of temporary closures.

SEQUENCE OF CONSTRUCTION

- 1.) Install all temporary soil erosion control measures.
- 2.) Install new fire hydrant.
- 3.) Demolition of the auto wash and appurtenances.
- 4.) Construct new convenience store and drive-thru restaurant building.
- 5.) Demolish existing convenience store and modify fuel island canopy.
- 6.) Install underground detention facility and storm sewer system.
- 7.) Install new pump island and complete interior paving/parking.
- 8.) Perform final grading per plans.
- 9.) Install all landscaping including seeding and sod.
- 10.) Remove temporary SESC measures.

Due to the nature of this project and the intention to remain operational during construction it is necessary to deviate from the standard practice of constructing the detention facility first. The existing pavement will remain in place during construction of the new store/restaurant and soil erosion will be minimized because of that. Soil erosion control measures will be maintained during construction.

All costs associated with the project are the responsibility of the owner and are estimated to be \$800k.

COMMUNITY ANALYSIS

This project will have no impact on public schools or surrounding developments. There are no natural features or historical sites or structures at this location. The project will be adding storm water detention which is not currently part of this site and therefore will be lessening the burden on the City's storm sewer system.

TRAFFIC IMPACT

The changes proposed will have minimal impact on the traffic flow and will continue to provide adequate maneuverability for vehicles on site.

SOIL TYPES

The majority of the site has Morley Loam @ 2%-6% slopes (MoB). The eastern 20% of the site has Matherton sandy loam @ 0%-4% slopes (MdA).

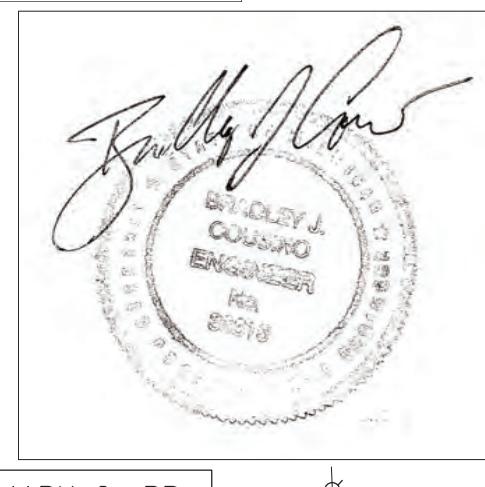
EX. STORM SEWER	ST	PROP. TELEPHONE	UGT
EX. SANITARY SEWER	SAN	OVERHEAD LINES	DTE OH
EX. WATER MAIN	W	FENCE	x x x x x
EX. GAS LINE	GAS	EXISTING SPOT ELEVATION	x 000.0
EX. UNDERGROUND ELECTRIC	E	PROPOSED SPOT ELEVATION	x 000.0
EX. UNDERGROUND TELEPHONE	UGT	TOP OF CURB	x T/C = 000.0
PROP. STORM SEWER	ST	EXISTING PAVEMENT REMOVED	grid
PROP. SANITARY SEWER	SAN		
PROP. WATER MAIN	W		
PROP. GAS LINE	GAS		
PROP. UNDERGROUND ELECTRIC	E		

LEGAL DESCRIPTION:

Part of the Southwest 1/4, Town 3 South, Range 6 East, City of Ann Arbor, Washtenaw County, Michigan, Commencing at the Southwest corner of Section 4; thence North 1 degree 53 minutes West 75 feet to point of beginning; thence continuing North 1 degree 53 minutes West 175 feet; thence North 87 degrees 0 minutes 0 seconds 210 feet to point of beginning; thence South 1 degree 53 minutes East 175 feet; thence South 87 degrees 0 minutes 0 seconds 210 feet to point of beginning; thence North 87 degrees 9 minutes 00 seconds 158.23 feet and North 87 degrees 9 minutes 00 seconds 158.15 feet from the Southwest corner of said Section 4, thence North 2 degrees 3 minutes 13 seconds West 27 feet; thence North 87 degrees 9 minutes 00 seconds 158.23 feet; thence South 01 degrees 53 minutes 00 seconds East 27 feet; thence South 87 degrees 9 minutes 00 seconds West 158.15 feet to the point of beginning.

More commonly known as: 2991 South State Street, Ann Arbor, MI 48104

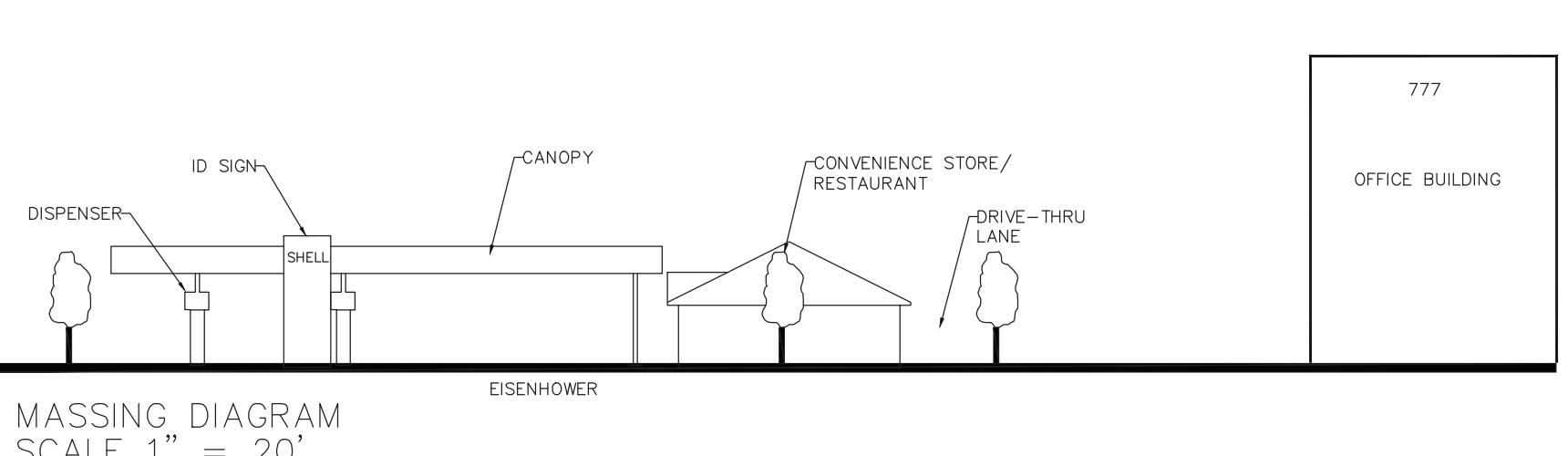
Tax item No. 12-04-300-048



PROJECT INFORMATION		
CODE REFERENCE -	2009 MICHIGAN BUILDING CODE	
ICC / ANSI A117.1 - 2003		
ZONING -	C3 COMMUNITY CONVENIENCE	
USE GROUP -	M MERCANTILE	
OCCUPANT LOAD	90	
CONSTRUCTION TYPE -	3B FIRE SUPPRESSION NONE	

THIS PROJECT WILL BE DEVELOPED IN ONE PHASE.

SHEET INDEX	
SHEET	DESCRIPTION
1	TS-1 TITLE SHEET & DIMENSIONAL SITE PLAN
2	DEM-1 DEMOLITION PLAN
3	SD-1 SITE ENGINEERING AND SOIL EROSION PLAN
4	LS-1 LANDSCAPE PLAN
5	A-1 FLOOR PLAN
6	A-2 FRONT & RIGHT SIDE ELEVATIONS
7	A-3 REAR & LEFT SIDE ELEVATIONS
8	PH-1 PHOTOMETRIC PLAN
9	DET-1 DETAILS
10	UT-1



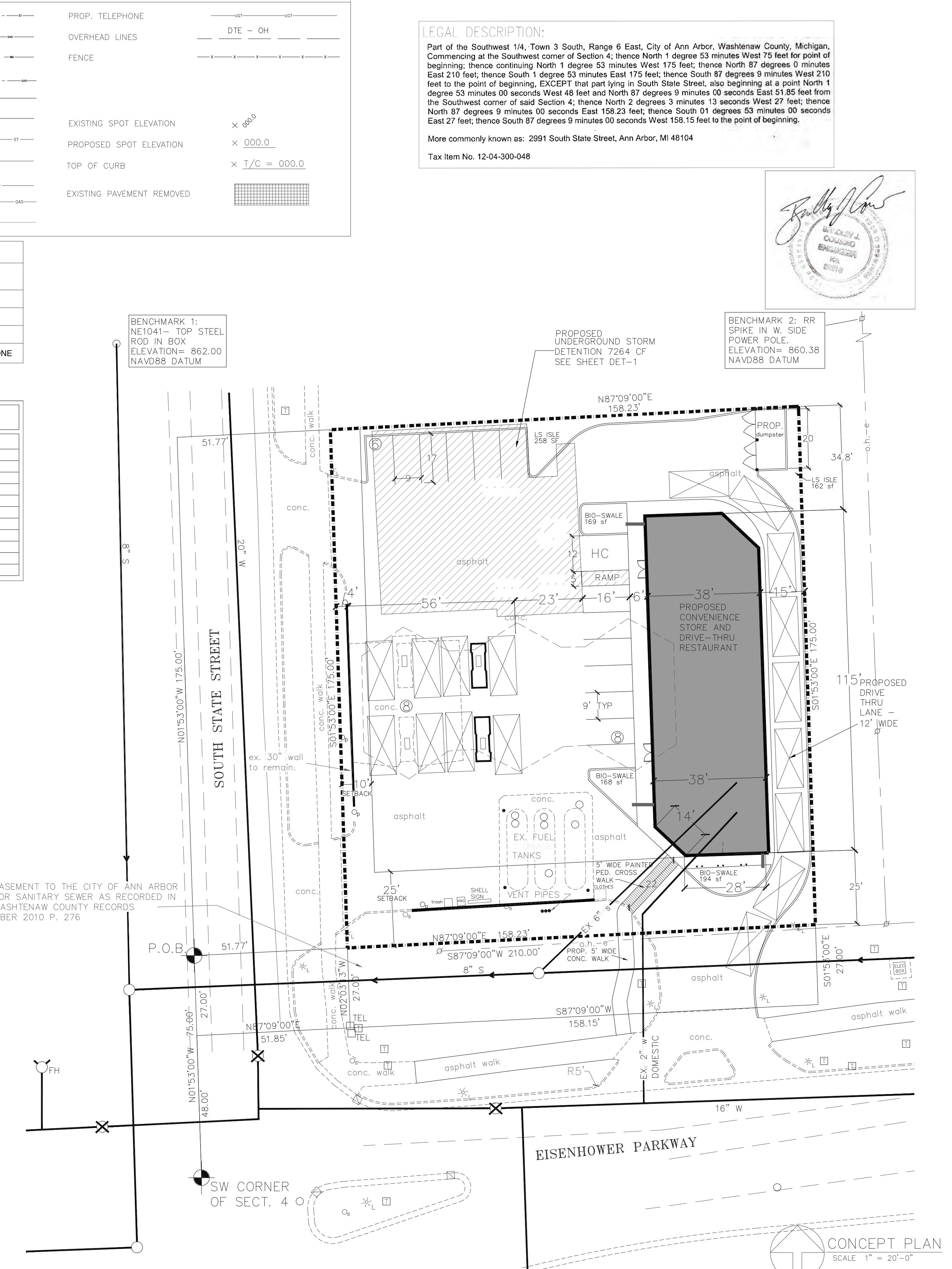
NOTES:
- ALL WORK SHALL BE IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE CITY OF ANN ARBOR
- EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO ANY WORK STARTING ON THIS SITE
- EXISTING WATER, SANITARY SEWER AND STORM SEWER SYSTEMS SHALL BE UTILIZED

COMPARISON CHART

ITEM	EXISTING	PROPOSED	PERMITTED/REQUIRED
ZONING	C3	C3	N/A
ADJACENT ZONING	O	N/A	
SITE AREA	27,686 SF	27,686 SF	
BUILDING FLOOR AREA	CARWASH 1675 SF EX. STORE 760.5 SF	NEW BLDG. 4250 SF	6000 SF MIN. 13,843 SF
FLOOR AREA RATIO	8.8%	15.4%	50%
BUILDING HEIGHT	12'	22'	35'
PARKING SPACE	14	21 + 9 STACKING	24
BICYCLE SPACES	2	3	3
SETBACKS			
FRONT (EISENHOWER)	19.87' 50'	25' 105.2'	10' MIN. - 25' MAX.
FRONT (STATE ST.)	2.5'	15'	10' MIN. - 25' MAX.
SIDE (OPPOSITE STATE ST.)	48.33'	34.80'	0
REAR (OPP. EISENHOWER)			0

SITE DATA

LOT AREA	27,686 SQ. FT. (0.636 ACRE)	EXISTING BUILDING AREA (TO BE DEMOLISHED)	2,435.5 SQ. FT.
MAX. LOT COVERAGE	150%	PROPOSED BUILDING AREA	4,250 SQ. FT.
PROPOSED LOT COVERAGE	15.4%	TOTAL BUILDING AREA	4,250 SQ. FT.
PROPOSED LANDSCAPE AREA	8%	MAX. BUILDING HEIGHT	35 FT.
EXISTING PARKING PROVIDED	10 SPACES	PROPOSED BUILDING HEIGHT	30 FT.
PARKING REQUIRED			
RESTAURANTS NOT IN A RETAIL CENTER		BICYCLES	
CAR PARKING		RESTAURANT - 1364/750 = BICYCLE PARKING REQUIRED = 1.82 => 2.0	
ONE SPACE PER 100 SQ. FT. OF FLOOR AREA MAX. 1364 SQ. FT. / 100 SQ. FT. = 14 SPACES		RESTAURANT - 1364/750 = BICYCLE PARKING REQUIRED = 1.82 => 2.0	
PARKING REQUIRED		VEHICLE PARKING STALL	
RESTAURANTS LESS THAN 30,000 SQ. FT.		9' WIDE X 18' DEPTH (16' LONG, IF 2' OF OVERHANG IS PROVIDED WHEN PARKING AGAINST A CURB)	
CAR PARKING		BICYCLE PARKING STALL	
ONE SPACE PER 310 SQ. FT. OF FLOOR AREA MAX. 2250 SQ. FT. / 310 SQ. FT. = 7.26 OR 8 SPACES		2' WIDE X 6' DEPTH W/ 3' MIN. CLEAR ACCESS AISLE WIDTH	
TOTAL REQUIRED - 22			
VEHICULAR PARKING PROVIDED - 22			



Job Location:
SHELL GAS STATION
2991 S. State St.
ANN ARBOR, MI

Owner:
JAK Cubed, LLC
248.797.6242

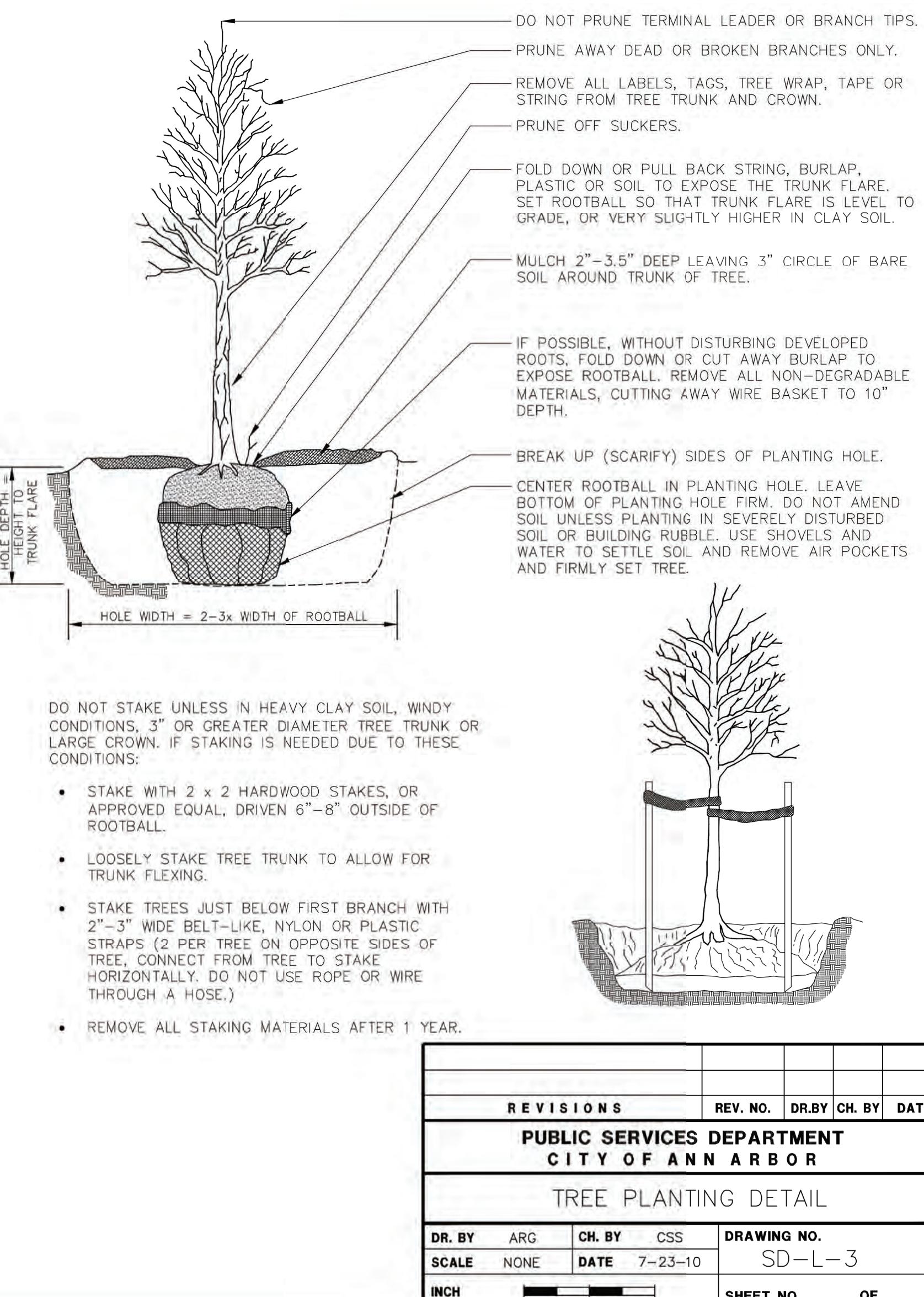
TERRATEK DESIGN, INC.
2860 TEEPEYAC HILL DR. ANNARBOR, MI 48105
PH. 734.320.4626
EMAIL: brad.cousino@live.com

JOB NUMBER
13-013

LANDSCAPE PLAN

REVISED DATE
2/24/14

LS-1



LANDSCAPE REQUIREMENTS:

INTERIOR LANDSCAPE REQUIREMENTS:

REQUIRED:
1 S.F. INTERIOR LANDSCAPING FOR EA. 20 S.F. OF VEHICULAR USE AREA.
LANDSCAPING MUST BE WITHIN INTERIOR OF VEHICULAR AREA, OR WITHIN THE CORNERS OF THIS AREA, AND MUST HAVE 1 DECID. TREE MIN. PER AREA.
18.904 S.F. VEHICULAR USE AREA / 20 = 945 S.F. LANDSCAPING REQUIRED.
** THE VEHICULAR USE AREA IS ALL PARKING AND DRIVING LANES ON THE SITE VIRTUALLY ALL PAVEMENT AS SHOWN, EXCLUSIVE OF THE BUILDING AND SURROUNDING WALKS.
50% OF INT. LANDSCAPE ISLANDS TO BE BIORETENTION AREAS 945/2=472.5 SF MIN.
PROVIDED:
LANDSCAPE AREAS TOTALING 951 S.F.,
REQUIRED INTERIOR TREE PLANTING: 1 PER 250 REQ'D LS => 945/250 = 3.78 => 4 REQUIRED/ PROVIDED.
3 BIORETENTION AREAS TOTALING 531 SF PROVIDED.

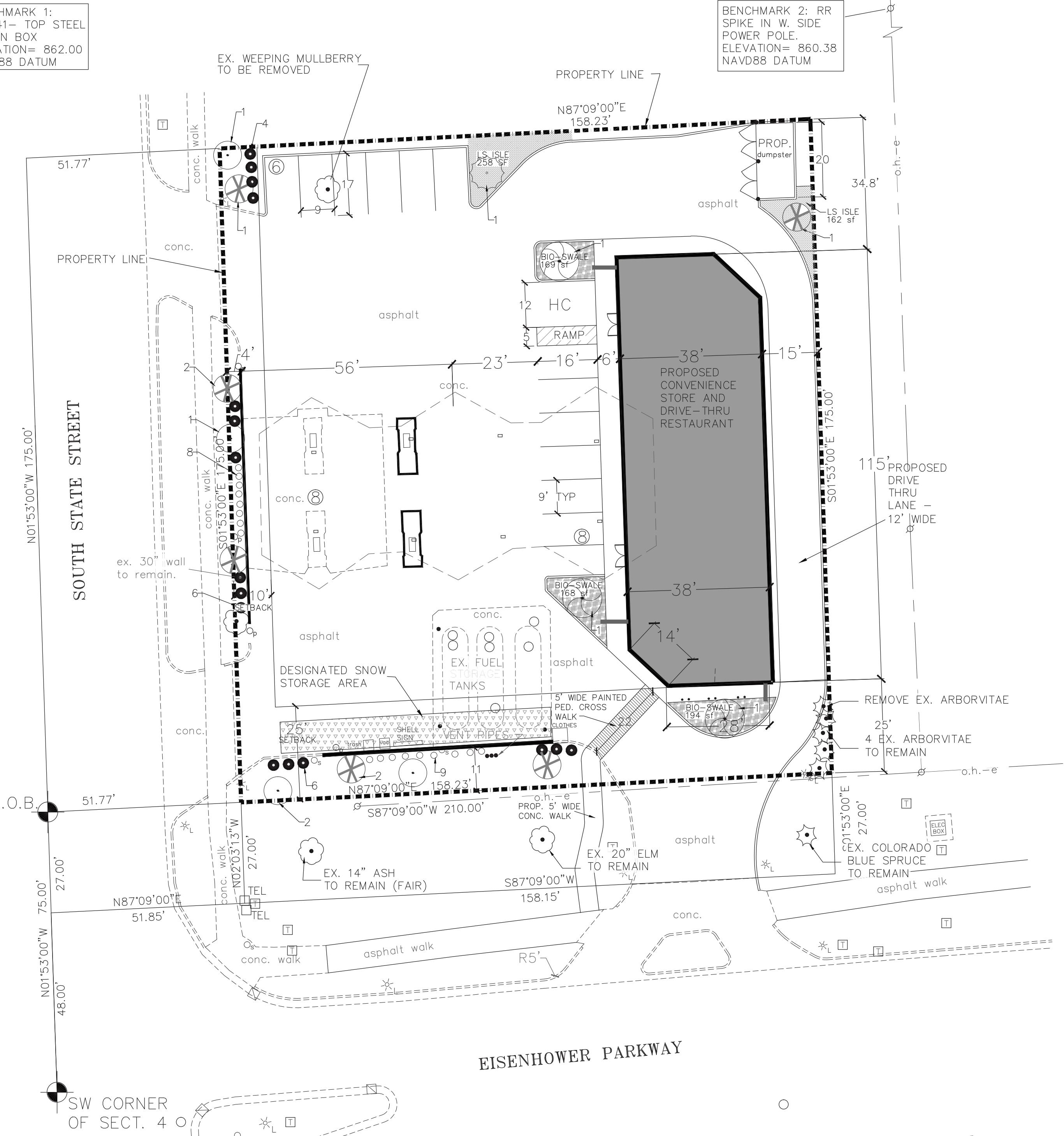
R.O.W. SCREENING REQUIREMENTS:

REQUIRED:
VEHICULAR USE AREAS MUST BE SCREENED FROM PUBLIC R.O.W. WITH A LANDSCAPED BUFFER STRIP AT LEAST 10' IN WIDTH, WITH 1 DECID. OR E.G. TREE PER 30 LF. OF FRONTAGE. A HEDGE, BERM, OR WALL MUST ALSO BE PROVIDED.
333 L.F. FRONTRAGE / 30 = 11 TREES REQUIRED.
333 L.F. FRONTRAGE / 10 = 33 SHRUBS REQUIRED.
EXISTING 30' WALLS AND ONE NEW WALL PROVIDES SCREENING ALONG FRONTAGES.

LANDSCAPE MODIFICATION REQUESTED ALONG STATE ST. FRONTRAGE:
PER CHAPTER 62, 5:608 (c) (ii) AND (c)(vii) - STRICT ADHERENCE OF 5:602(1)(a) (LANDSCAPE BUFFER STRIP) WOULD RESULT IN LOSS OF PARKING REQUIRED PARKING SPACES AND IS AN EXISTING NONCONFORMING CONDITION IN A PREVIOUSLY APPROVED SITE PLAN. AN EXISTING 30' WALL IS LOCATED 4' FROM THE STATE ST. ROW AND NEW SHRUBS ON THE STREET SIDE ARE PROPOSED. THIS MEETS 5:602(1)(d) SPECIFICALLY FOR GASOLINE STATIONS.

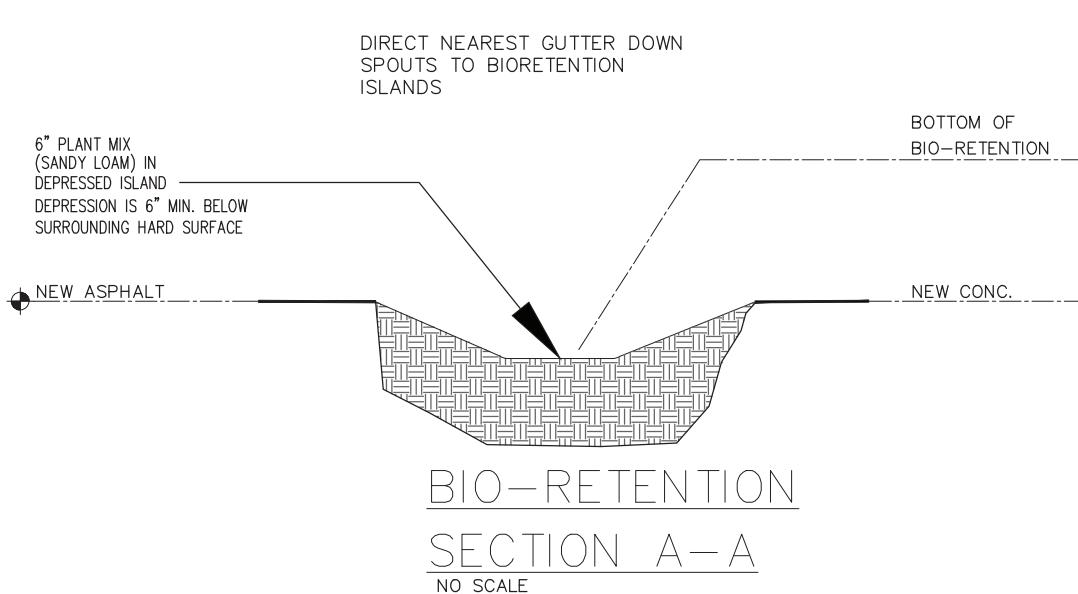
LANDSCAPE MATERIAL LIST

	QTY	BOTANICAL AND COMMON NAMES	SIZE
•	1	WHITE SPRUCE (<i>abies glauca</i>)	7' high B & B
○	6	SWEETGUM (<i>liquidambar styraciflua</i>)	2" cal.
●	3	RIVER BIRCH (<i>Betula nigra</i>)	2" cal.
●	16	NORDIC HOLLY (<i>ilex glabra 'nordi'</i>)	5 gal. pot
○	17	RED SPRITE HOLLY (<i>ilex verticillata "Red Sprite"</i>)	24" HEIGHT
●	4	BLACK GUM (<i>"Nyssa sylvatica"</i>)	2" CAL.



LANDSCAPE GENERAL NOTES

- The location of existing or proposed utilities are shown for general locations only. The Contractor shall determine the exact location of all utilities before commencing work, and agrees to be fully responsible for any and all damages which might be occasioned by the Contractor's failure to exactly locate and preserve any and all utilities. If conflicts of a proposed site improvement occurs with any utility, the Contractor shall notify the Landscape Architect of such conflict prior to relocating the site improvement for review of the condition. The proposed element in conflict shall not be installed until written approval is provided by the office of the Architect. CONTACT MISS-DIG 1-800-482-7171 prior to any work.
- Trees and plant material on the approved site plan cannot be removed without approval by Public Service area administrator/designee or appropriate approving body.
- All disturbed areas shall be restored with 3" deep topsoil and sodded with Michigan State Seed Company Grade 'A' sod mix. The entire site shall be irrigated with an automatic underground irrigation system.
- All landscape improvements shall be installed by a Landscape Contractor familiar with the trade. The installation of plant materials must consider any site conditions that may require special soil modifications or planting depths. Plant substitutions must be approved by Public Service Area administrator/designee or appropriate approving body.
- All mulch shall be Hardwood shredded bark, installed 4" deep unless otherwise indicated. All planting bed areas in which shrubs are installed shall be mulched as a mass bed area. Landscape commercial grade black vinyl edging shall be installed at all locations where mulch meets lawn areas. All soils within planting beds or root ball zone areas shall have Preen, Treflan or equal applied to the soil (per manuf. spec.) prior to installation of shredded bark mulch.
- All plant materials shall be maintained regularly in a healthy and growing state. Materials that are unsightly, dead, dying, or that become unhealthy because of damage, neglect, drainage problems, disease, insect infestation, or other causes shall be replaced within one year.
- Earth berms shall not exceed one foot vertical for each three feet horizontal and shall have at least a two feet wide generally flat top.
- All plant materials and lawn areas shall be installed and maintained in accordance with the City of Ann Arbor Ordinance requirements.
- All diseased, damaged or dead materials shown on the site plan shall be replaced by the end of the following growing season.
- All trees will be planted at least 15 feet center to center.
- Replacement trees will be planted within 30 days of completion of hardscape construction on site, or as soon as planting conditions allow if construction is completed during winter conditions.
- Any application of fertilizer beyond the additional topsoil and seeding shall be a fertilizer with no phosphorus.
- Compacted soil areas to be treated on site:
 - Existing weedy vegetation to be removed
 - 4" topsoil to be applied to surface
 - Rototill topsoil to a depth of 8"
- New landscape islands on site to be provided with 6" sandy loam planting medium minimum.
- Snow shall not be pushed onto interior landscape islands unless designated for snow storage.
- Roof downspouts in the vicinity of the bioretention swales shall outlet under the sidewalk and daylight/discharge into the bioretention swale. The swale shall be depressed to receive the roof drainage and shall have a depressed curb at the opposite end from the downspout for outlet purposes.

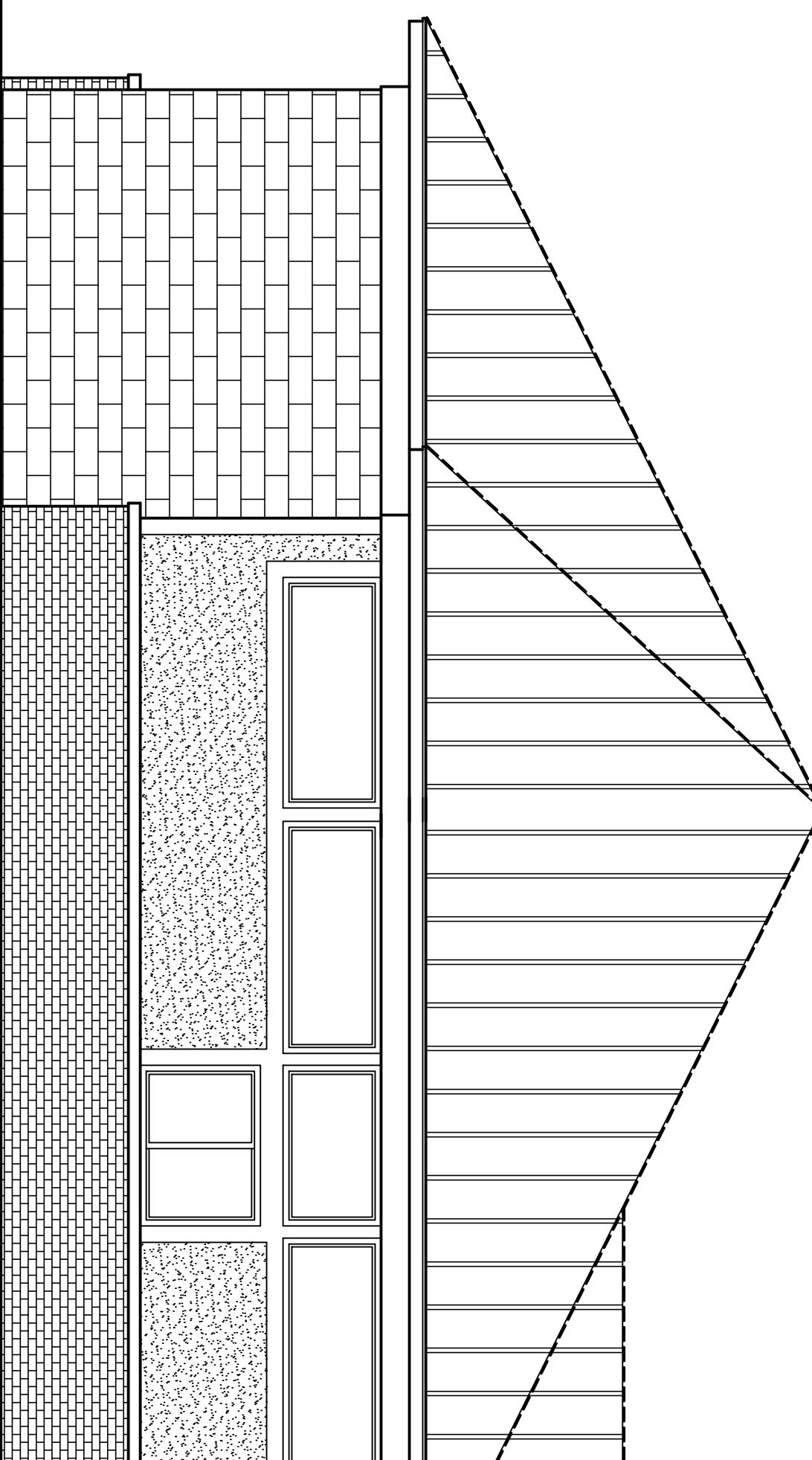


LANDSCAPE PLAN
SCALE 1" = 20'-0"
NORTH

NORTH (LEFT) ELEVATION

SCALE: 1/4"=1'-0"

PRELIMINARY -- NOT FOR CONSTRUCTION



EXTERIOR ELEVATIONS

SHEET SCALE
AS NOTED

PROJECT NO.
13-063

DATE
09.24.13

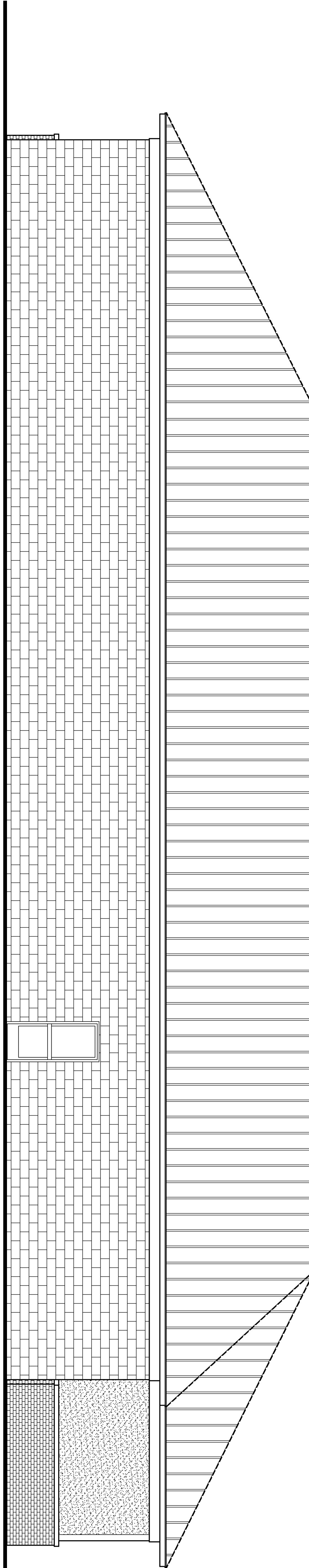
SHEET NUMBER
A-3

GAS STATION/DRIVE-THRU RESTAURANT

2991 SOUTH STATE STREET
ANN ARBOR, MICHIGAN 48104

ZACK M OSTROFF & ASSOCIATES
RESIDENTIAL/COMMERCIAL DESIGNER/PLANNER
web: www.zoarchitecture.com
email: zack@zoarchitecture.com
PH: 248.425.4190
FAX: 248.366.7122

MEMBER
AIA
BID



EAST (REAR) ELEVATION

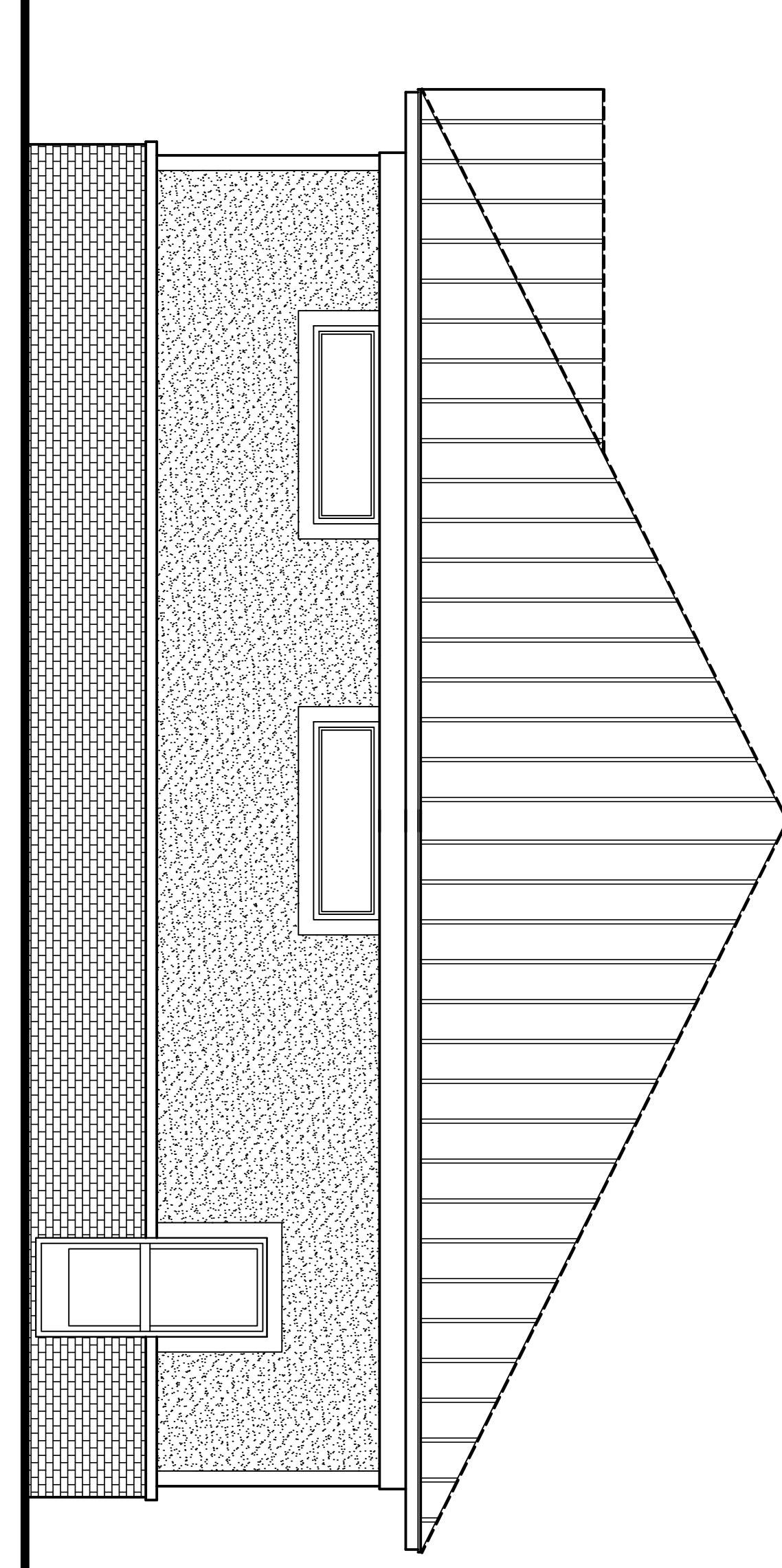
SCALE: 1/4"=1'-0"

REVISIONS
No. _____
Date _____
Addendum/Bulletin _____

PRELIMINARY -- NOT FOR CONSTRUCTION

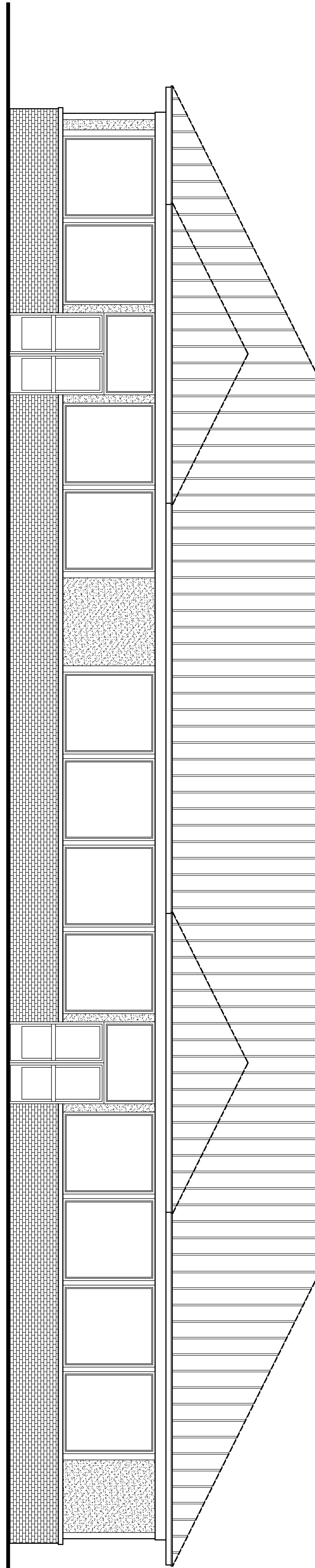
SOUTH (RIGHT) ELEVATION

SCALE: 1/4"=1'-0"



WEST (FRONT) ELEVATION

SCALE: 1/4"=1'-0"



EXTERIOR ELEVATIONS

Sheet Scale
AS NOTED

Project No.
13-063

Date
09.24.13

Sheet Number
A-2

ZACK M OSTROFF & ASSOCIATES
RESIDENTIAL/COMMERCIAL DESIGNER/PLANNER
web: www.zoarchitecture.com
email: zack@zoarchitecture.com
PH.248.425.4190
FAX.248.366.7122
MEMBER
AIA
BID

GAS STATION/DRIVE-THRU RESTAURANT

2991 SOUTH STATE STREET
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